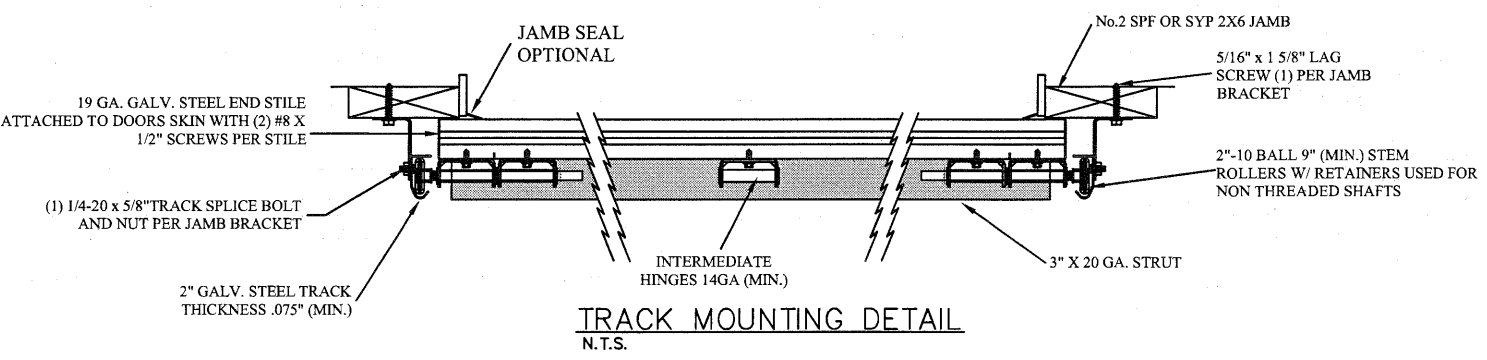
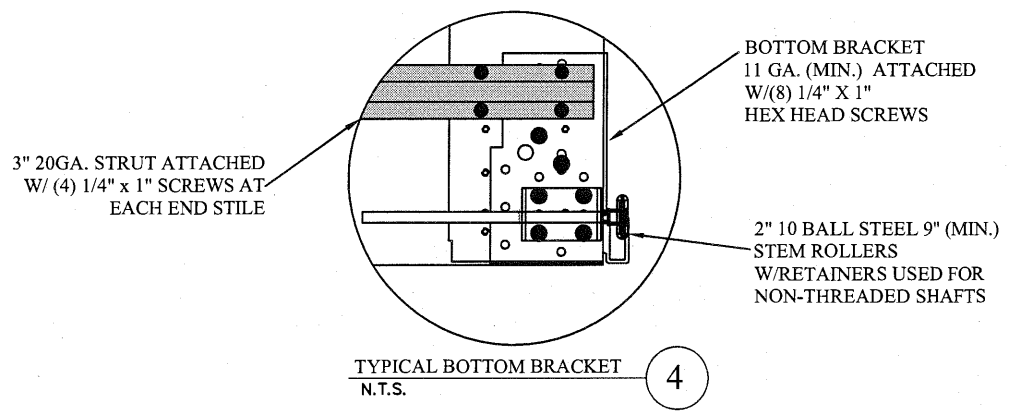
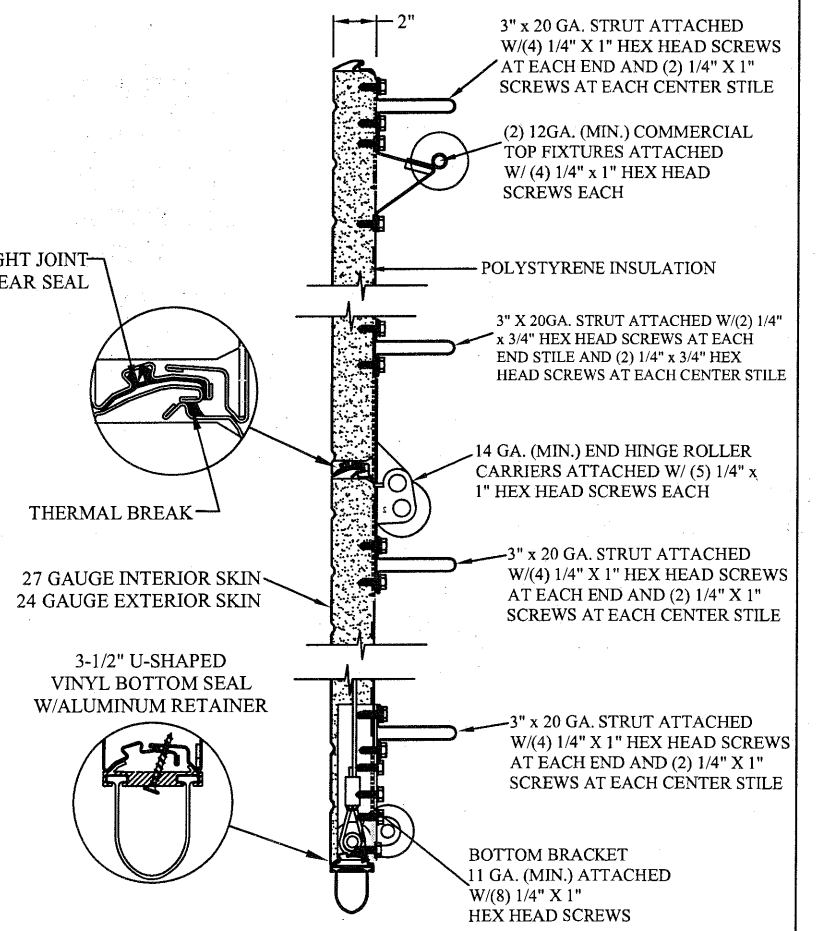


DOOR INTERIOR ELEVATION
N.T.S.



EDGE OF DOOR 1" OVERLAP ON EACH SIDE



THE METHOD OF TESTING WAS IN SUBSTANTIAL CONFORMANCE WITH THE PROCEDURES DESCRIBED IN DASMA 108. THE PRESSURES SHOWN ON THE DRAWINGS WERE CALCULATED USING ASCE 7-98/02/05 WITH THE FOLLOWING PARAMETERS (5 FEET OF DOOR WIDTH IN END ZONE, ROOF SLOPE 10' OR LESS, AND I=1.0):

| | | | | | |
|------------------|-----|-----|-----|-----|-----|
| WIND SPEED (MPH) | 151 | 137 | 130 | 124 | 119 |
| EXPOSURE LEVEL | B | C | C | D | D |
| MEAN ROOF HEIGHT | 30' | 15' | 25' | 15' | 25' |

| REV | DESCRIPTION OF REVISIONS | DATE | BY |
|-----|--------------------------|------|----|
| | | | |

MAX SIZE 14'2 x 24'

DESIGN LOADS
+31.5 PSF
-35.9 PSF

TEST LOADS
+47.3 PSF
-53.9 PSF

Thomas L. Shelmerdine, PE (TX PE #85829)
Structural Solutions, PA (TX Firm #004063)

STATE OF TEXAS
THOMAS L. SHELMERDINE
85829
LICENSED PROFESSIONAL ENGINEER

TX

Amarr
ENTREMATICS

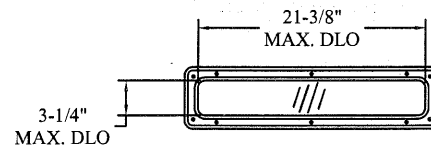
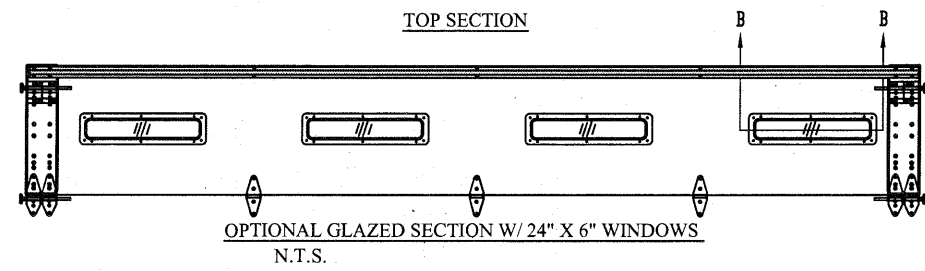
MODEL 1000 AMARR 2432

| | | | | | |
|------|------------|-----|------|--------|-----------------|
| SIZE | DRAWN BY | RLR | DATE | 6/6/17 | DRAWING NUMBER |
| B | CHECKED BY | RLR | DATE | 6/6/17 | IBC-1014-150-15 |

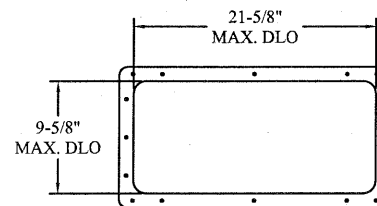
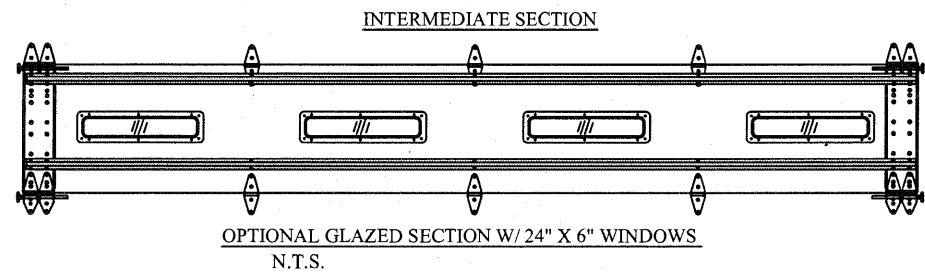
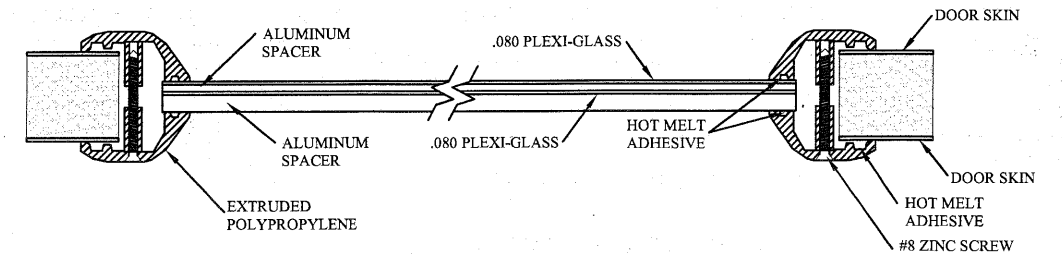
ENTREMATICS
165 CARRIAGE COURT WINSTON-SALEM, N.C. 27105

SHEET 1 OF 4

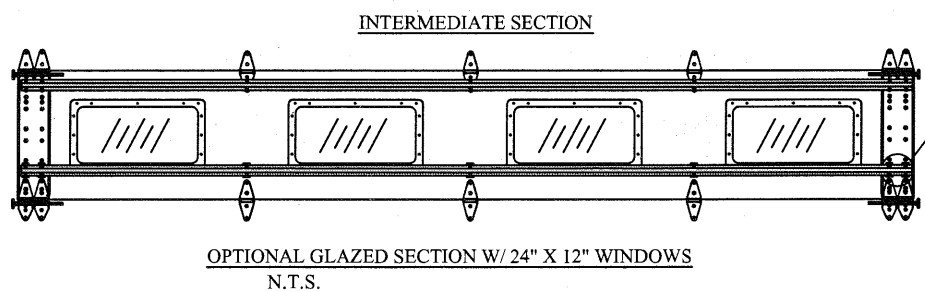
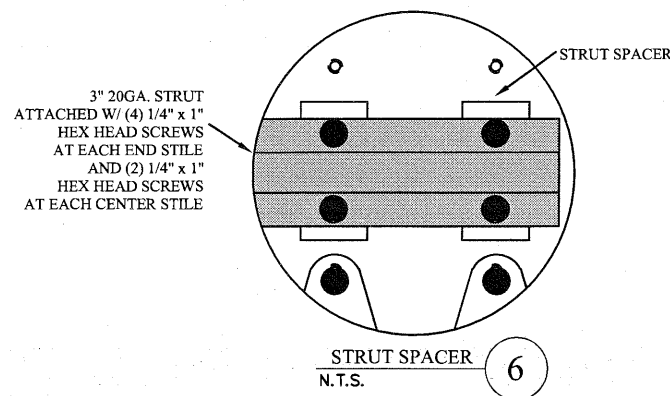
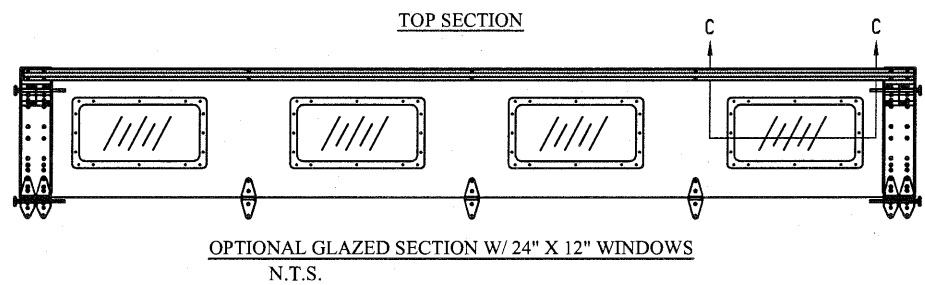
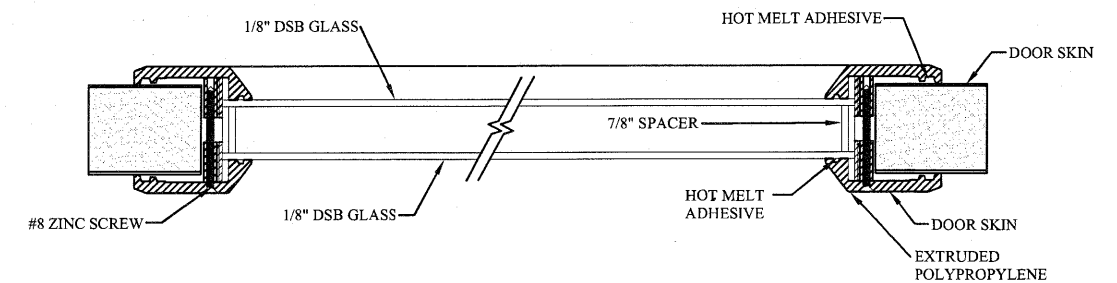
5921-G W. Friendly Ave., Greensboro, NC 27410



24" X 6" DAY LIGHT OPENING AND FASTENER DETAIL
N.T.S.



24" X 12" DAY LIGHT OPENING AND FASTENER DETAIL
N.T.S.



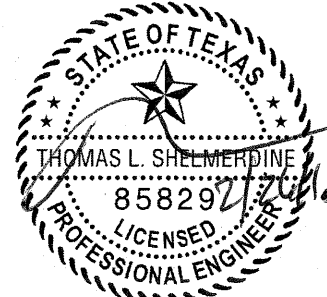

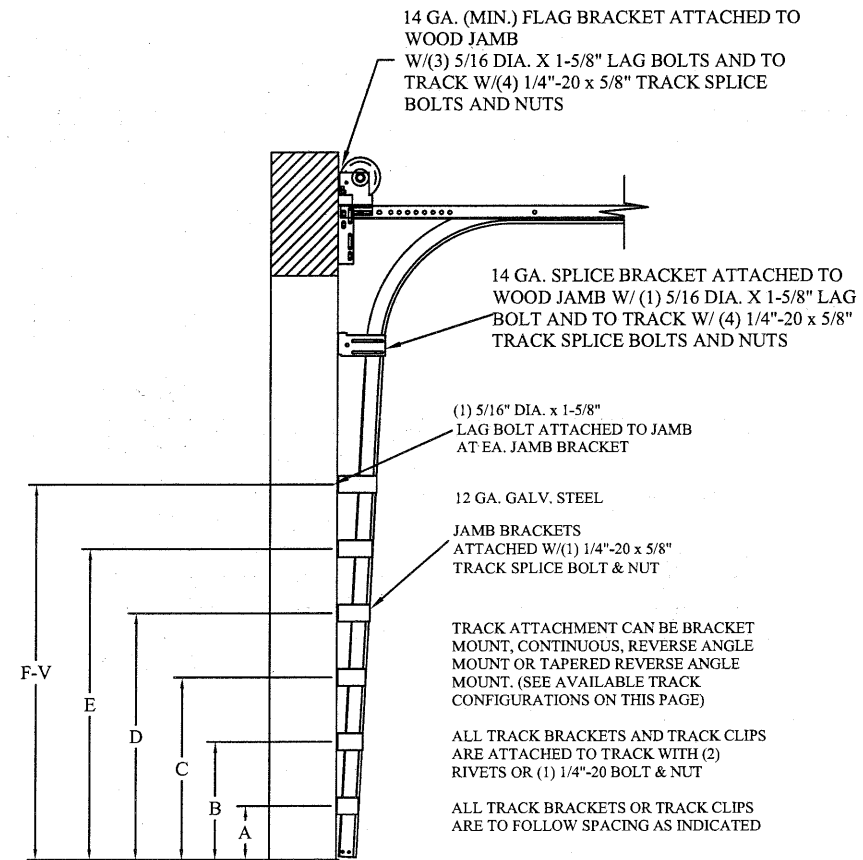
| REV | DESCRIPTION OF REVISIONS | DATE | BY | | | | | | | | | | | | | | | |
|--|---|------|----------|--------|----------------|--------|----------------|---|------------|-----|------|--------|--|--|----------------|--|-----------------|--|
| | | | | | | | | | | | | | | | | | | |
| | MAX SIZE 14'2" x 24' | | | | | | | | | | | | | | | | | |
| | DESIGN LOADS +31.5 PSF -35.9 PSF | | | | | | | | | | | | | | | | | |
| | TEST LOADS +47.3 PSF -53.9 PSF | | | | | | | | | | | | | | | | | |
| | <div style="display: flex; align-items: center;"> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-size: 8px; margin-right: 5px;"> Thomas L. Shelmerdine, PE (TX PE #85829) Structural Solutions, PA (TX Firm #F-004063) </div> <div style="text-align: center;">  <p>TX</p> </div> <div style="writing-mode: vertical-rl; transform: rotate(180deg); font-size: 8px; margin-left: 5px;"> 5921-G W. Friendly Ave., Greensboro, NC 27410 </div> </div> | | | | | | | | | | | | | | | | | |
| |  | | | | | | | | | | | | | | | | | |
| | MODEL 1000 AMARR 2432 | | | | | | | | | | | | | | | | | |
| | <table border="1" style="font-size: 8px;"> <tr> <td>SIZE</td> <td>DRAWN BY</td> <td>RLR</td> <td>DATE</td> <td>6/6/17</td> <td rowspan="2">DRAWING NUMBER</td> </tr> <tr> <td>B</td> <td>CHECKED BY</td> <td>RLR</td> <td>DATE</td> <td>6/6/17</td> </tr> </table> | SIZE | DRAWN BY | RLR | DATE | 6/6/17 | DRAWING NUMBER | B | CHECKED BY | RLR | DATE | 6/6/17 | <table border="1" style="font-size: 8px;"> <tr> <td colspan="2">DRAWING NUMBER</td> </tr> <tr> <td colspan="2" style="text-align: center;">IBC-1014-150-15</td> </tr> </table> | | DRAWING NUMBER | | IBC-1014-150-15 | |
| SIZE | DRAWN BY | RLR | DATE | 6/6/17 | DRAWING NUMBER | | | | | | | | | | | | | |
| B | CHECKED BY | RLR | DATE | 6/6/17 | | | | | | | | | | | | | | |
| DRAWING NUMBER | | | | | | | | | | | | | | | | | | |
| IBC-1014-150-15 | | | | | | | | | | | | | | | | | | |
| ENTREMATIC 165 CARRIAGE COURT WINSTON-SALEM, N.C. 27105 | | | | | | | | | | | | | | | | | | |
| SHEET 3 OF 4 | | | | | | | | | | | | | | | | | | |

TABLE 1

| Section Width (ft) | Center Stile Locations (Measured from Left) | | | Max Design Loads Allowed | |
|--------------------|---|----------|----------|--------------------------|----------------|
| | 1st (in) | 2nd (in) | 3rd (in) | Positive (PSF) | Negative (PSF) |
| 6' 0" | 36" | - | - | 37.1 | 42.3 |
| 6' 2" | 37" | - | - | 36.1 | 41.2 |
| 6' 4" | 38" | - | - | 35.2 | 40.1 |
| 6' 6" | 39" | - | - | 34.3 | 39.1 |
| 6' 8" | 40" | - | - | 33.4 | 38.1 |
| 6' 10" | 41" | - | - | 32.6 | 37.2 |
| 7' 0" | 42" | - | - | 31.8 | 36.3 |
| 9' 4" | 36" | 76" | - | 35.0 | 39.9 |
| 9' 6" | 37" | 77" | - | 34.5 | 39.4 |
| 9' 8" | 38" | 78" | - | 34.1 | 38.9 |
| 9' 10" | 39" | 79" | - | 33.7 | 38.4 |
| 10' 0" | 40" | 80" | - | 33.3 | 37.9 |
| 10' 2" | 41" | 81" | - | 32.8 | 37.4 |
| 10' 4" | 42" | 82" | - | 32.4 | 37.0 |
| 10' 6" | 43" | 83" | - | 32.0 | 36.5 |
| 10' 8" | 44" | 84" | - | 31.7 | 36.1 |
| 13' 0" | 36" | 78" | 120" | 31.7 | 36.1 |
| 13' 2" | 37" | 79" | 121" | 31.7 | 36.1 |
| 13' 4" | 38" | 80" | 122" | 31.7 | 36.1 |
| 13' 6" | 39" | 81" | 123" | 31.7 | 36.1 |
| 13' 8" | 40" | 82" | 124" | 31.7 | 36.1 |
| 13' 10" | 41" | 83" | 125" | 31.7 | 36.1 |
| 14' 0" | 42" | 84" | 126" | 31.7 | 36.1 |
| 14' 2" | 43" | 85" | 127" | 31.5 | 35.9 |

* CONTACT ENGINEERING FOR SIZES 7'2" THROUGH 9'2" & 10'10" THROUGH 12'10"

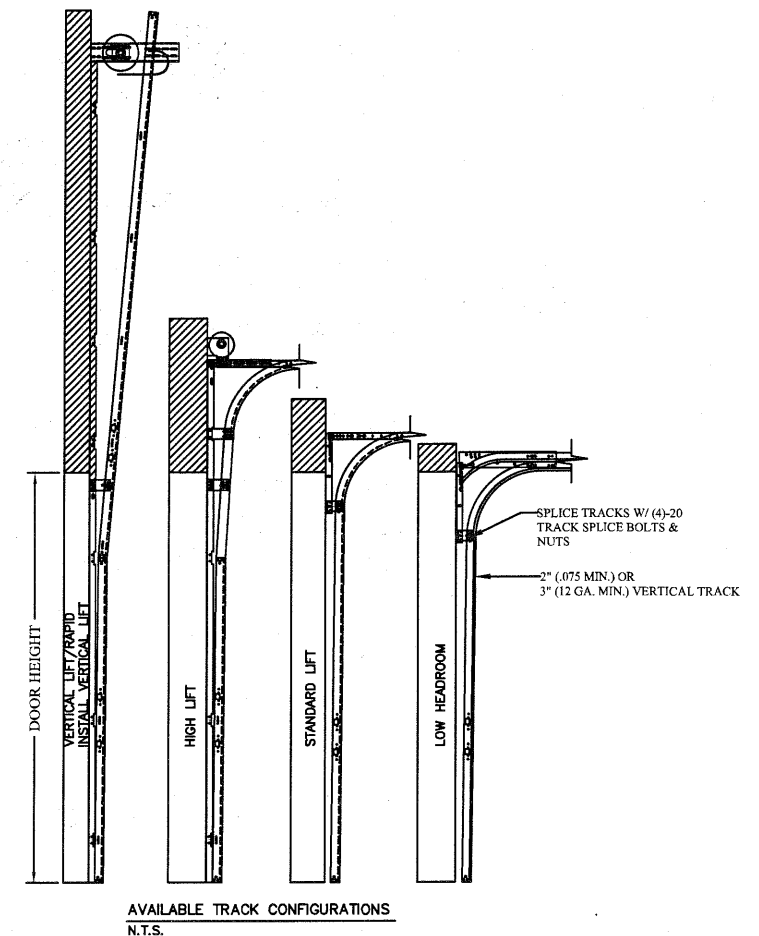


TRACK CONFIGURATION FOR UP TO 24' TALL DOORS
SEE TABLE 2

TABLE 2

| DOOR HEIGHT | TRACK ATTACHMENT | | | | | | | | | | | | | | | | | | | | TYPICAL SPLICE | | |
|-------------|------------------|-------|-----|-----|-----|-----|-----|-----|------|------|------|------|------|------|------|------|------|------|------|------|----------------|------|------|
| | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | | U | V |
| 7' 0" | 10.0" | 22.0" | 34" | 46" | 58" | | | | | | | | | | | | | | | | | | 76" |
| 7' 6" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | | | | | | | | | | | | | | | | | 82" |
| 8' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | | | | | | | | | | | | | | | | | 88" |
| 8' 6" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | | | | | | | | | | | | | | | | 94" |
| 9' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | | | | | | | | | | | | | | | | 100" |
| 9' 6" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | | | | | | | | | | | | | | | 106" |
| 10' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | | | | | | | | | | | | | | 112" |
| 11' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | | | | | | | | | | | | | 124" |
| 12' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | | | | | | | | | | | | 136" |
| 13' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | | | | | | | | | | | 148" |
| 14' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | | | | | | | | | | 160" |
| 15' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | | | | | | | | | 172" |
| 16' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | | | | | | | | 184" |
| 17' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | | | | | | | 196" |
| 18' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | | | | | | 208" |
| 19' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | 214" | | | | | 220" |
| 20' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | 214" | 226" | | | | 232" |
| 21' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | 214" | 226" | 238" | | | 244" |
| 22' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | 214" | 226" | 238" | 250" | | 256" |
| 23' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | 214" | 226" | 238" | 250" | 262" | 268" |
| 24' 0" | 10.0" | 22.0" | 34" | 46" | 58" | 70" | 82" | 94" | 106" | 118" | 130" | 142" | 154" | 166" | 178" | 190" | 202" | 214" | 226" | 238" | 250" | 262" | 280" |

ALL TRACK ATTACHMENTS +/- 2" ALLOWED USING SYP OR SPF NO.2 OR BETTER ONLY



AVAILABLE TRACK CONFIGURATIONS
N.T.S.

| REV | DESCRIPTION OF REVISIONS | DATE | BY |
|-----|--------------------------|------|----|
| | | | |

MAX SIZE
14'2 x 24'

DESIGN LOADS
+31.5 PSF
-35.9 PSF

TEST LOADS
+47.3 PSF
-53.9 PSF

Thomas L. Shelmerdine, PE (TX PE #85829)
Structural Solutions, PA (TX Firm #F-0040663)

TX

5921-G W. Friendly Ave., Greensboro, NC 27410

MODEL 1000 AMARR 2432

| | | | | | |
|--|------------|-----|------|--------|-----------------|
| SIZE | DRAWN BY | RLR | DATE | 6/6/17 | DRAWING NUMBER |
| B | CHECKED BY | RLR | DATE | 6/6/17 | IBC-1014-150-15 |
| ENTREMATIC | | | | | SHEET 4 OF 4 |
| 165 CARRIAGE COURT WINSTON-SALEM, N.C. 27105 | | | | | |